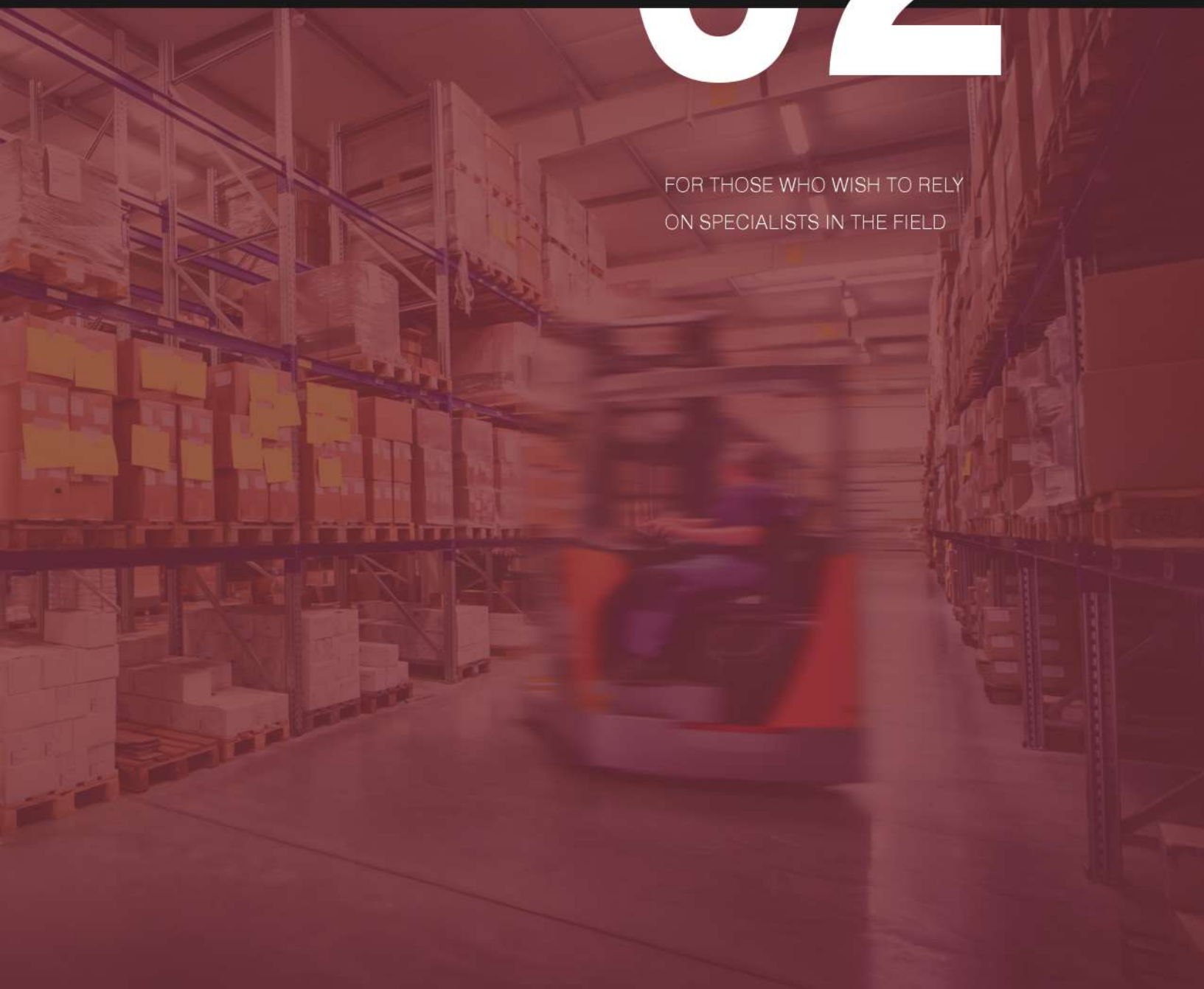


# 02

LOADING BAYS

FOR THOSE WHO WISH TO RELY  
ON SPECIALISTS IN THE FIELD









**We know the needs of technicians all over  
the world:**

# 50


**YEARS EXPERIENCE**

Exporting our innovative products to a network of over 50 countries showcases the seamless fusion of Italian technology and design that defines our brand's global presence and success.

A photograph of a modern industrial building with a grey and yellow facade. Several loading bays are open, revealing white roll-up doors. The building is situated on a concrete surface with white parking lines. The text is overlaid in the center of the image.

**THE  
LOADING BAYS  
ARE BORN TO  
CONNECT NATIONS AND CULTURE  
THROUGH TRADE**





Loading bays are essential for loading and unloading process.  
PFI produces complete loading bays, generally consisting of:

- Dock levelers;
- Dock shelters;
- Sectional doors;

Dock levelers, combined with sectional doors, are an obligatory solution, for companies that need to make the frequent loading and unloading operations.

They are the fastest and more efficient systems for loading and unloading operations.

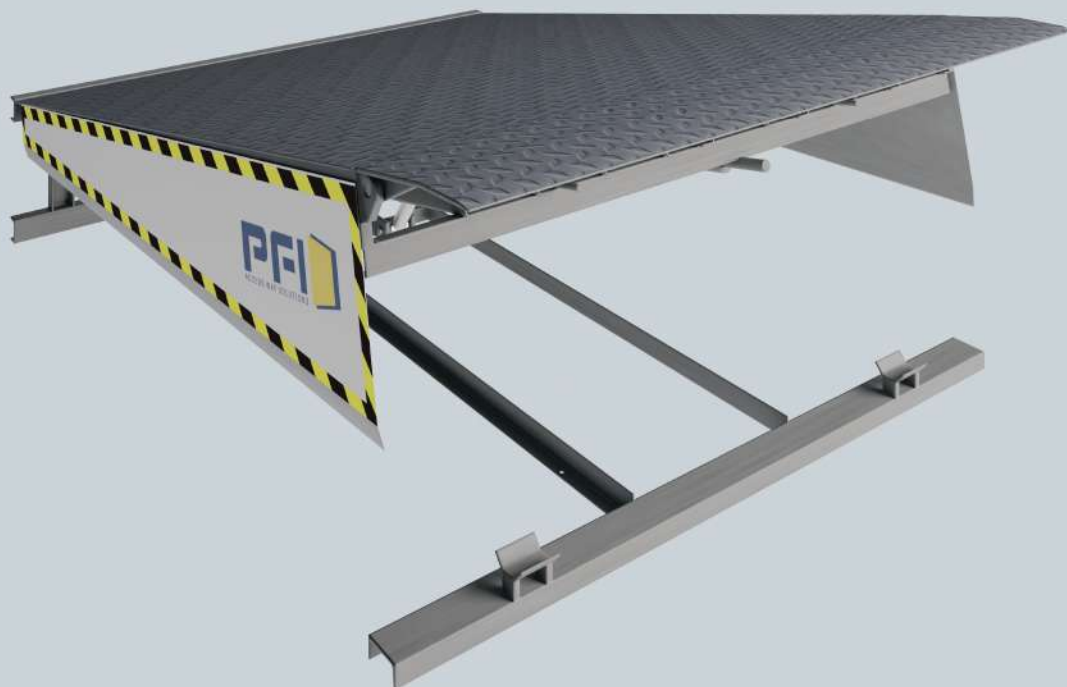
It is possible to install a dock shelter, outside the structure, in order to isolate the warehouse from outside, to protect the goods from the weather conditions during loading / unloading phases and to protect the health of the staff, as they are very effective even against current of air.

Our loading bays are the optimum solution to regulate the work flow and traffic of goods to save energy.

DOCK LEVELER

# SWINGING LIP

Swinging lip hydraulic dock levelers are the most used type in loading and unloading operations.



## / TECHNICAL SPECIFICATIONS

**6.000<sup>kg</sup>**

Standard capacity

**400<sup>mm</sup>**

Lip length

## / BASIC SPECIFICATIONS

Platform thickness

**6+2<sup>mm</sup>**

Lip thickness

**12/13<sup>mm</sup>**

Vertical working range

**+640 / -300<sup>mm</sup>**

Standard color

**7016<sup>RAL</sup>**

Maximum width

**2.200<sup>mm</sup>**

Maximum length

**3.000<sup>mm</sup>**

The dock leveler structure consists of lip, platform and frame which is made of painted galvanized steel sheet.

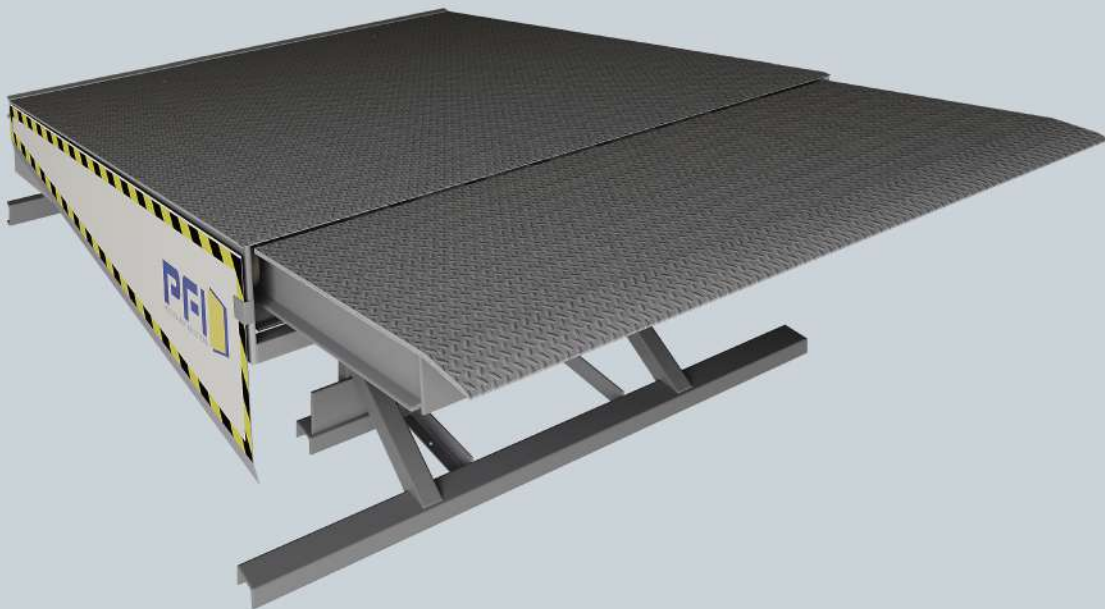
The main platform and the lip are made from SD37 grade galvanized steel sheet.

It performs the lifting and lowering operation of the main platform by using a single piston placed under the platform.

DOCK LEVELER

# TELESCOPIC LIP

Telescopic lip hydraulic dock levelers are the most suitable solution for places where high thermal insulation is required.





## / TECHNICAL SPECIFICATIONS

**6.000<sup>kg</sup>**

Standard capacity

**1.000<sup>mm</sup>**

Lip length

## / BASIC SPECIFICATIONS

Platform thickness

**8+2<sup>mm</sup>**

Lip thickness

**12/13<sup>mm</sup>**

Vertical working range

**+640 / -300<sup>mm</sup>**

Standard color

**7016<sup>RAL</sup>**

Maximum width

**2.200<sup>mm</sup>**

Maximum length

**3.000<sup>mm</sup>**

The dock leveler structure consists of lip, platform and frame which is made of painted galvanized steel sheet.

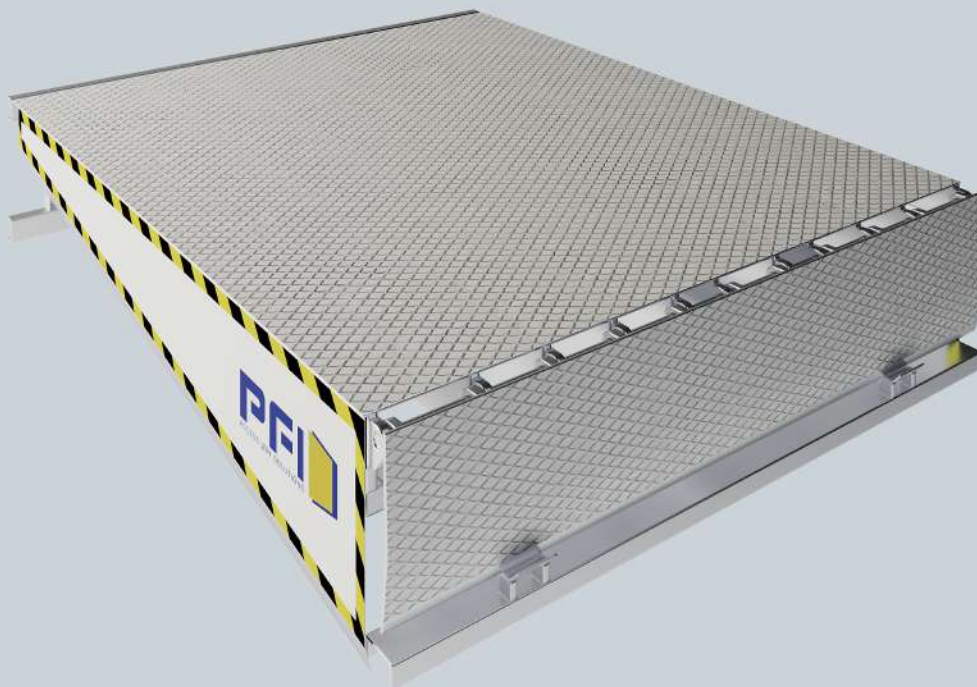
The main platform and the lip are made from SD37 grade galvanized steel sheet.

It performs the lifting and lowering operation of the main platform by using two pistons placed under the platform.

DOCK LEVELER

# GALVANIZED STEEL

Hot-dip galvanizing is a useful process for creating a robust barrier against oxidation and prolonging the life of the dock leveler.



**GALVANIZED STEEL DOCK LEVELER**

# **GALVANIZED TO LAST FOREVER. THE STRENGTH THAT SUPPORTS EVERY STEP**

PFI produces a totally hot-dip galvanized steel version.

In order to guarantee excellent galvanisation on every detail of the dock leveler, we have developed a product that does not require any welding. In fact, this version is made up of a frame and a fully bolted platform.

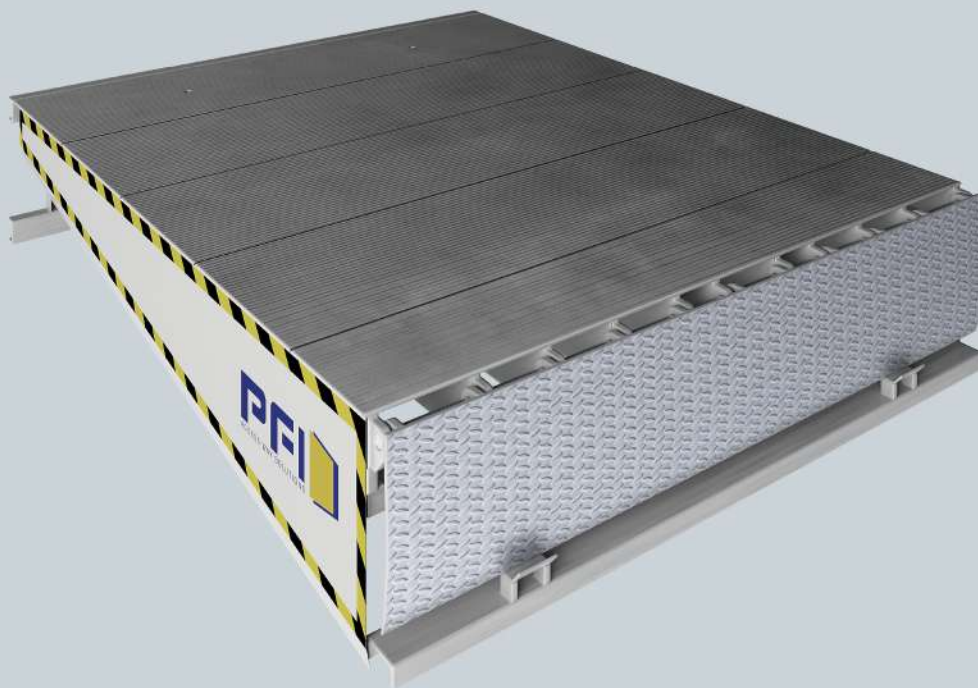


# Aluminum dock levelers

**PFI produces aluminum hydraulic dock leveler. The advantages of using aluminum compared to steel are: the total absence of rust formation and simplified transport, due to the small size of the product.**

The structure of our aluminum dock levelers, with swinging lip, is provided with a forklift-eye, so that the pit installation is facilitated by the forklift.

Thanks to the use of aluminum, it provides more durability over time than standard dock levelers since it does not require surface finishes (paint galvanizing) and does not rust.



## / TECHNICAL SPECIFICATIONS

**6.000**<sup>kg</sup>

Capacity

**370**<sup>mm</sup>

Lip length

## / BASIC SPECIFICATIONS

Platform

**40**<sup>mm (H)</sup>

Maximum load

**1,3**<sup>N/mm<sup>2</sup></sup>

Oil allowed

**Hydraulic 32**

Standard color

**Aluminum**

Maximum width

**2.200**<sup>mm</sup>

Maximum length

**3.000**<sup>mm</sup>

The dock leveler structure consists of lip, platform and frame.

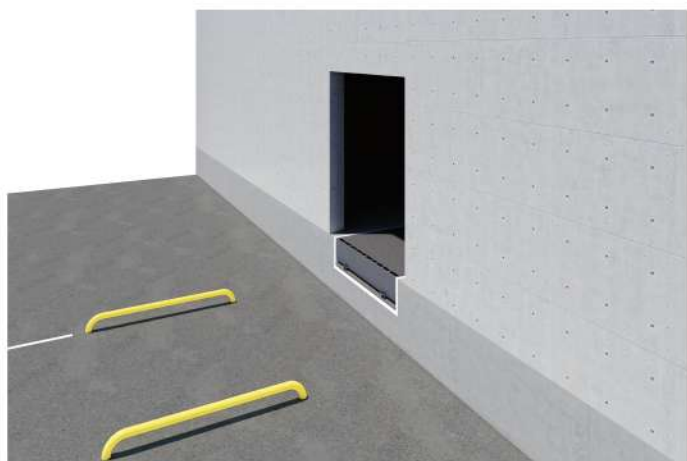
The platform and lip are made of extruded aluminium.

Upon request it is possible to have the lip in galvanized steel, with a total length of 400mm.

During assembly process, the pits for housing the platform must be prepared according to indications provided by PFI.

# ACCESSORIES

All dock levelers can be equipped with many accessories, useful for various applications



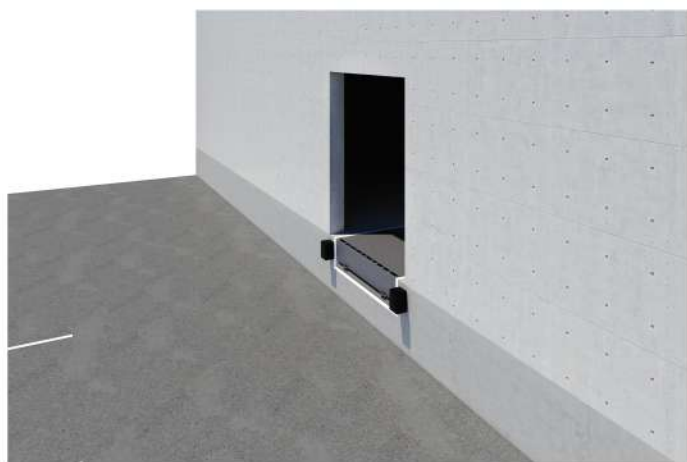
## ROUTING BARRIER

Installed at a distance equivalent to the width of the vehicles



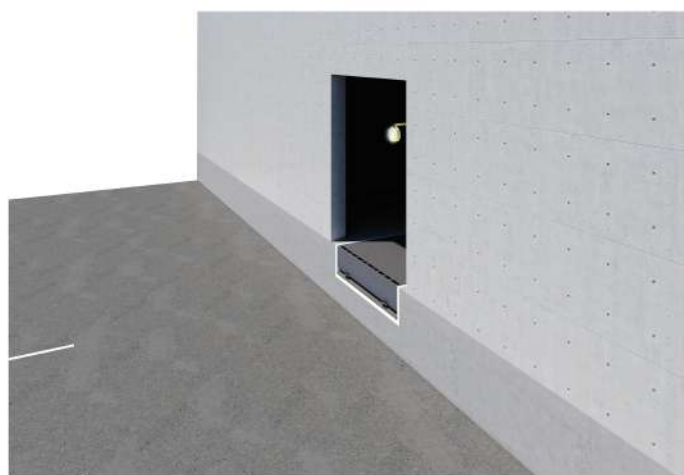
## WHEEL CHOCKS

Installed in the ground will block the vehicles



## BUMPERS

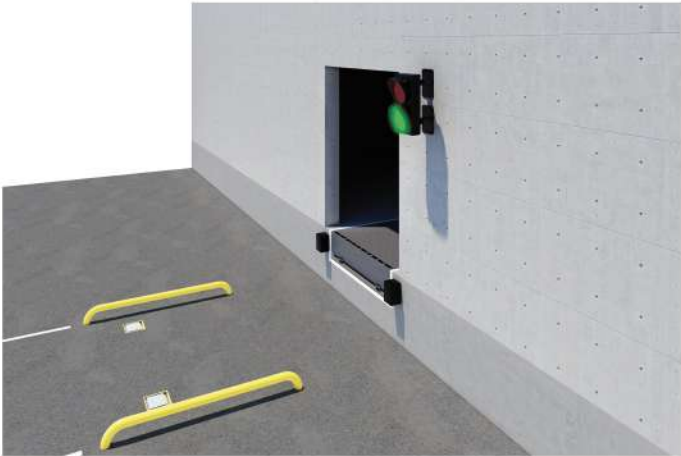
To prevent the vehicle from crashing the dock leveler



## LIGHTING SYSTEM

To increase visibility in the vehicles





## **TRAFFIC LIGHT**

To indicate whether the loading bay is free or occupied

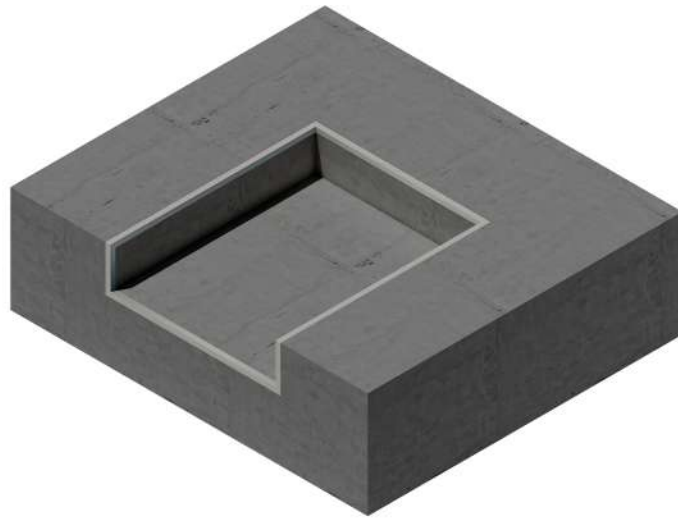
# Dock levelers easy installation

During assembly process, the pits for housing the platform must be prepared according to indications provided by PFI. The pit must be surrounded by L-shaped steel profiles (not included) firmly anchored to the concrete by means of clamps.

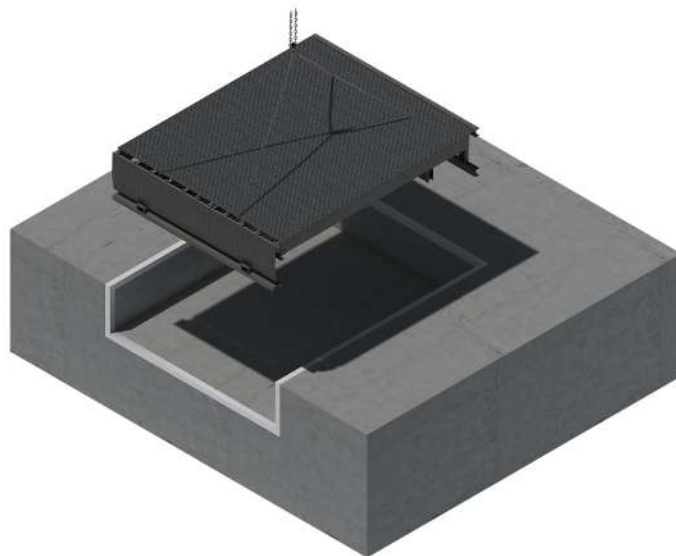
The dock leveler must be raised using the tie rods (not included), fixed in the appropriate anchors in the platform and inserted from above into the prepared pit.

Once correctly positioned, the dock leveler must be welded on the rear corners prepared in the pit.

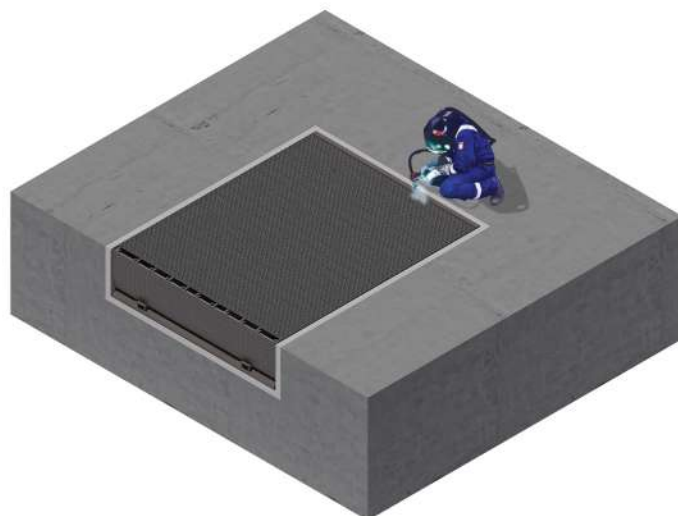




**Pit**



**Dock leveler installation**



**Welding**



We are producing three models of dock shelters for loading bays

# PROTECTIVE, INSULATING, RELIABLE

The dock shelters guarantee continuity to the cold chain.

They are designed to isolate the vehicle and loading area from air permeability and dust.

Depending on the needs and the type of use, three types of dock shelters are used: standard, cushions and inflatable.

The last two are particularly suitable for areas where thermal insulation is required.





DOCK SHELTER

# RETRACTILE

Standard dock shelters are commonly used where extremely high thermal insulation is not required.





/ TECHNICAL SPECIFICATIONS

**2,9<sup>mm</sup>**

Front sheet thickness

**900<sup>gr/m<sup>2</sup></sup>**

Perimeter sheet thickness

/ BASIC SPECIFICATIONS

Operation

**Manual**

Vertical sheets width

**700<sup>mm</sup>**

Horizontal sheet height

**1.000<sup>mm</sup>**

Standard color

**Black**

Width

**3.400<sup>mm</sup>**

Height

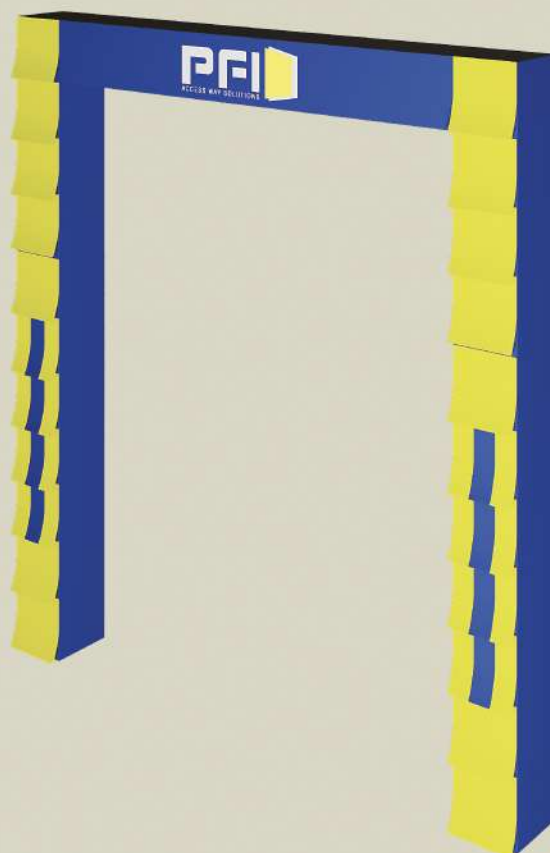
**3.500<sup>mm</sup>**

Standard dock shelter consists of an aluminum structure, anchored to the outer wall of the loading bay, a PVC sheet that surrounds the structure around the entire perimeter and black PVC coated flaps on the front, which will adhere to the truck.

DOCK SHELTER

# CUSHIONS

Cushions dock shelters, with their ability to keep the internal temperature stable, are often used in refrigerated rooms.



/ TECHNICAL SPECIFICATIONS

**32** kg/m<sup>3</sup>

Foam density

**680** gr/m<sup>2</sup>

Coating sheet thickness

/ BASIC SPECIFICATIONS

Operation

**Manual**

Upper cushion height

**300** mm

Lateral cushion width

**300** mm

Standard color

**Blue**

Maximum width

**3.000** mm

Maximum height

**3.000** mm

Isothermal vans, when placed in the loading compartment, provide excellent insulation by applying pressure on the cushions.

The cushions consist of a core of flexible foam covered with reinforced PVC.

They can be fixed or mobile if an isothermal van needs to be used on different dimensions.

DOCK SHELTER

# INFLATABLE

Inflatable dock shelters, with their ability to keep the internal temperature stable, are often used in refrigerated rooms.





## / TECHNICAL SPECIFICATIONS

**250<sup>W</sup>**

Absorption

**65<sup>dB</sup>**

Noise level

## / BASIC SPECIFICATIONS

Operation

**Automatic**

Bags fabric quality

**2.500<sup>g/m<sup>2</sup></sup>**

Top bags height

**1.300<sup>mm</sup>**

Inflation time

**8<sup>sec.</sup>**

Width

**3.600<sup>mm</sup>**

Height

**4.000<sup>mm</sup>**

Thanks to a fan on inflatable dock shelter, the blower inflates its wings until it clings to the walls of the vehicle.

This system is definitely the most practical and convenient solution because it automatically adapts to any size isothermal truck.







**FOR THOSE WHO WISH TO  
RELY ON SPECIALISTS IN  
THE FIELD**

INDUSTRIAL DOORS

# SECTIONAL DOORS

Sectional doors open by sliding vertically, which allows them to take up as little space as possible when opening.





## SECTIONAL DOORS

# SAFETY, STYLE, SOLIDITY. OPEN NEW PERSPECTIVES WITH OUR SECTIONAL DOORS

Thanks to the perimeter gaskets, they guarantee an optimal thermal insulation and resistance to atmospheric agents. They consist of galvanized steel rails, where panels with expanded polyurethane, covered with pre-painted sheet, hinged together, slide through shelves with wheels.

SECTIONAL DOORS

# STRUCTURE

Suitable for standard applications (panel thickness 40mm) and for refrigeration field (panel thickness 80mm)



/ TECHNICAL SPECIFICATIONS

**40/80<sup>mm</sup>**

Panel thickness

**I<sub>a</sub> 25<sup>dB</sup>**

Sound proof

/ BASIC SPECIFICATIONS

Wind resistance

**1000** <sup>Pa N/m<sup>3</sup></sup>  
Class 3

Air permeability

**40** <sup>m<sup>3</sup>/m<sup>2</sup>/h at 60 Pa</sup>  
Class 3

Thermal resistance <sup>(no windows)</sup>

**0,8** <sup>W / (m<sup>2</sup>/K)</sup>

Thermal resistance <sup>(with windows)</sup>

**1,0** <sup>W / (m<sup>2</sup>/K)</sup>

Maximum width

**6.000** <sup>mm</sup>

Maximum height

**6.000** <sup>mm</sup>

# Elevation types for sectional doors

**As standard, PFI offers different types of elevation, which allow the use of sectional doors with any lintel height.**

There are three most common elevations: standard lift, high lift and vertical lift.

The first one is recommended when the height of the lintel is limited to 500mm.

If the height of the lintel exceeds 500mm it is recommended to opt for the high lift models

Otherwise, when the height of the lintel is more than double the height of the door, it is advisable to opt with vertical lift.





## MODELS



## STANDARD

Lintel height = 500<sup>mm</sup>



## HIGH

Lintel height > 500<sup>mm</sup>

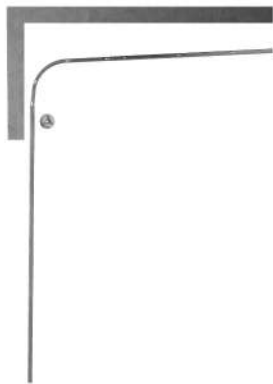


## VERTICAL

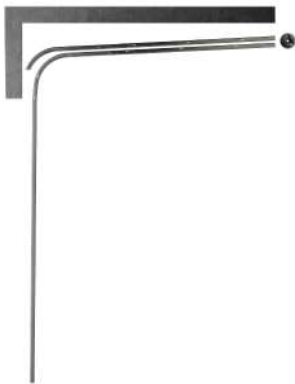
Lintel height = (H\*2) + 410<sup>mm</sup>

# ELEVATION TYPES

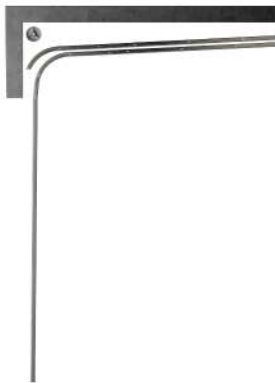
In order to satisfy any requirement, PFI promote other types of elevations, more elaborate than those previously described.



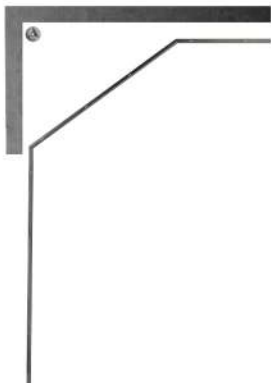
**PSDHS**  
High lift  
(spring on back of panel)



**PSDLB**  
Lower lift  
(spring on back)



**PSDLF**  
Lower lift  
(spring on front)



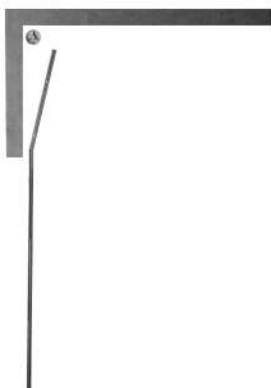
**PSDSW**  
High lift angular



**PSDLC**  
Sloping lower lift



**PSDSC**  
Sloping standard lift



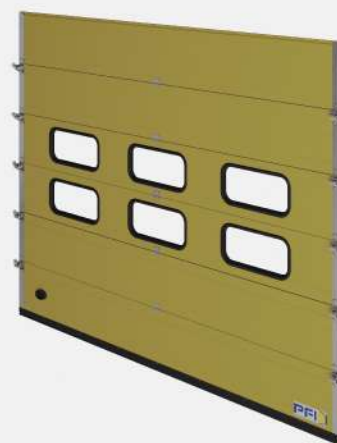
**PSDVA**  
Vertical lift angular



**PSDVS**  
Vertical lift  
(spring on back of panel)

SECTIONAL DOORS

# WINDOW OPTIONS





## SECTIONAL DOORS

**WINDOWS THAT LOOK OUT  
ONTO THE WORLD OPEN  
DOORS TO THE BEAUTY AND  
INFINITE DIVERSITY OF LIFE.**

To allow greater illumination and to improve visibility between different zones, the panels can be integrated with standard windows.

As standard, windows are not included in our sectional doors but can be optional.

# ACCESSORIES

All sectional doors can be equipped with many accessories, useful for various applications



## AUTOMATION

With motor and control panel



## MAGNETIC LOOP

It detect the presence of forklift and open the door



## PHOTOCELLS

The door opens automatically when the signal is interrupted



## PULL CORD

Allow the door to be opened by a rope hanging from above



## REMOTE CONTROL

It is possible to operate multiple doors with one remote control



## PEDESTRIAN DOOR

For areas with high traffic of pedestrians



## LATCH

It will prevent the door from opening manually

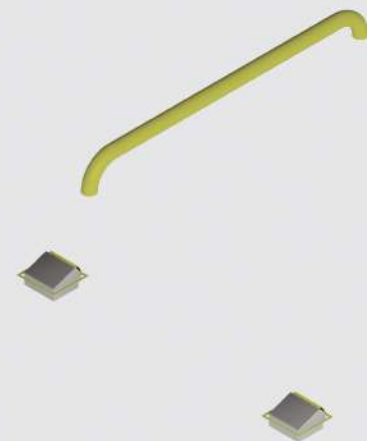
## LOADING BAYS: DESIGN GUIDE

The design of the loading bays, as well as their structure, is essential for logistics flows to take place in an agile and uncomplicated manner. The position of the loading bays is crucial for carrying out truck maneuvers both in terms of speed and in terms of safety.

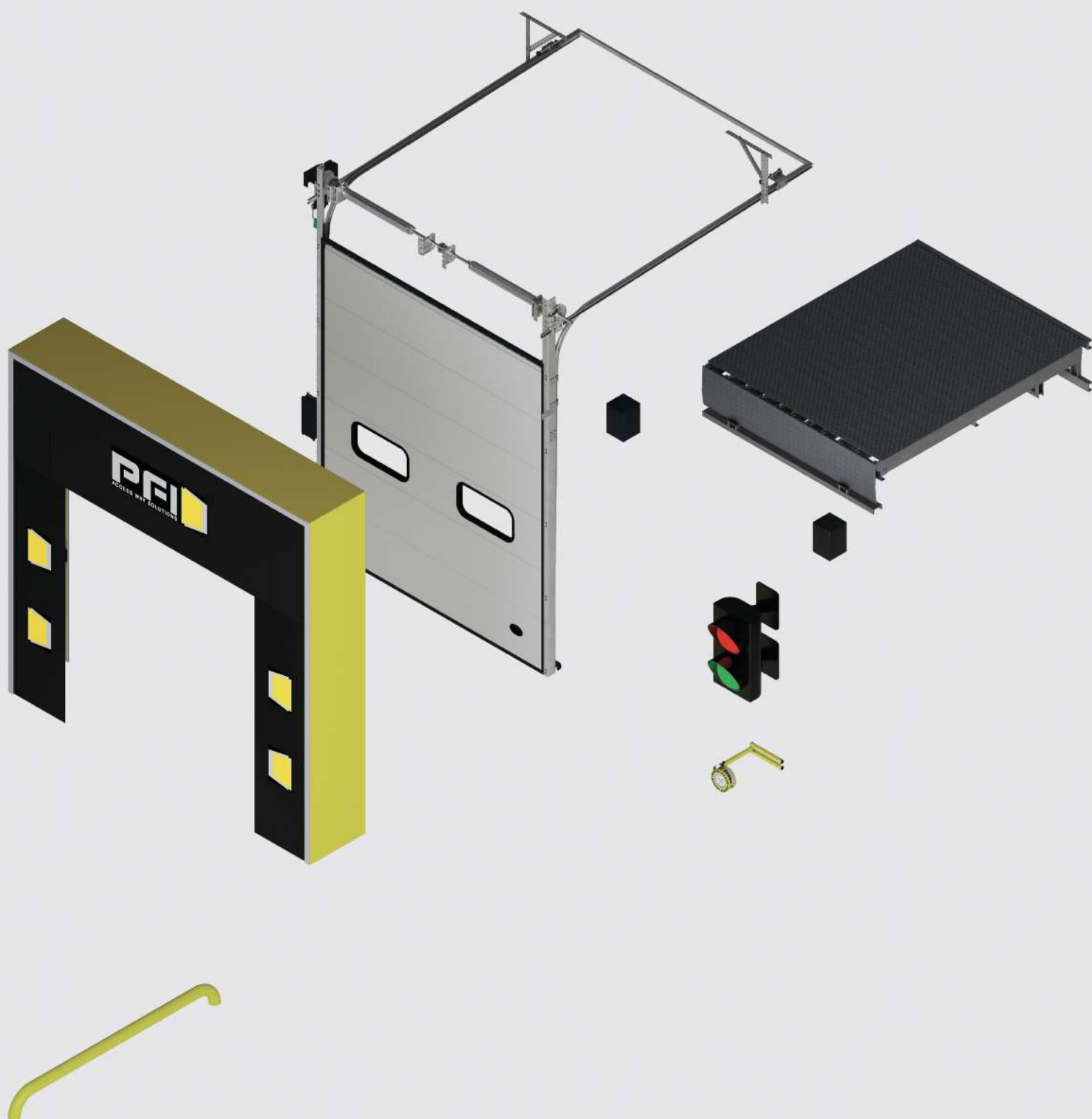
Given the importance they have on a logistical level and for warehouse activities, before building, it is a good idea to review this checklist.

- **Establish the warehouse approach;**
- **Decide whether you will receive large-capacity trucks;**
- **Check the presence or absence of slopes in the ground;**
- **Consider the aesthetic and organizational factor;**
- **Analyze production factors;**
- **Study climate factors;**
- **Plan for future warehouse expansion.**

The number of loading bays depends on the positions in which the accesses to the plant are located, which in turn is determined by the study of the flows of goods, the volume of deliveries, the moment in which the goods are received, the time necessary for unload and transfer all loads received at the destination and by the number of available handling vehicles.







## PFI Loading Bays: Optimizing Logistics Flow

Loading Bays are an essential part of logistics operations in any company that manages the movement of goods and products. The PFI company, with its reputation for efficiency and professionalism, is no exception in this aspect. PFI's Loading Bays represent a key element in the company's supply chain and are carefully designed to ensure a smooth logistics flow. PFI is a leading company in the logistics and transportation field. Its operations involve the transportation of goods of all types and sizes, from small packages to large-scale cargo.



Cargo management is crucial to the company, and loading bays are at the heart of this process.

In conclusion, PFI loading bays are a crucial part of our success in logistics and transportation. Their thoughtful design, efficient management and attention to safety help ensure that loading and unloading operations run smoothly. PFI constantly strives to improve its products and remain at the forefront of the industry, making the loading bays a key selling point for our company.







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